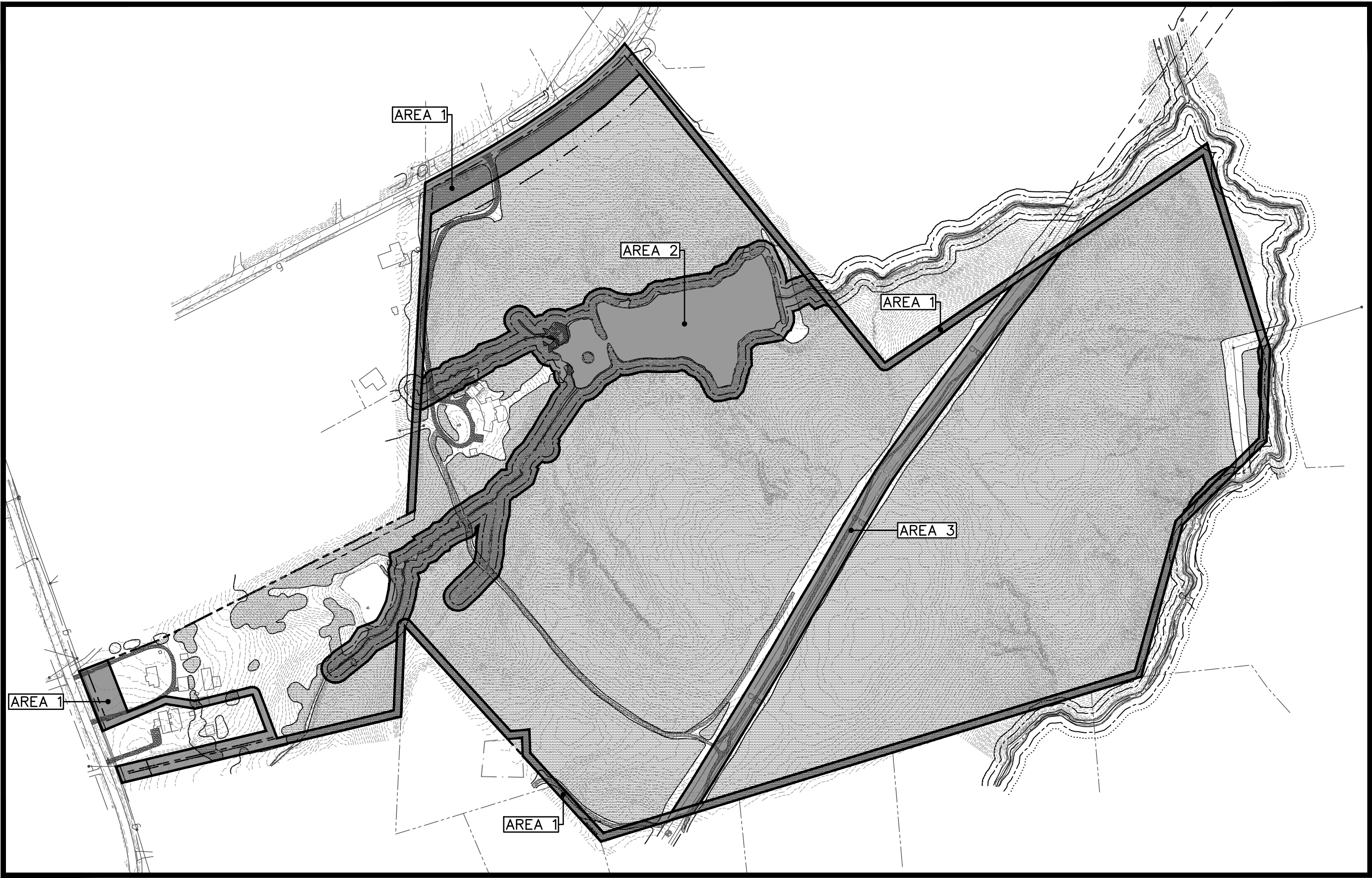


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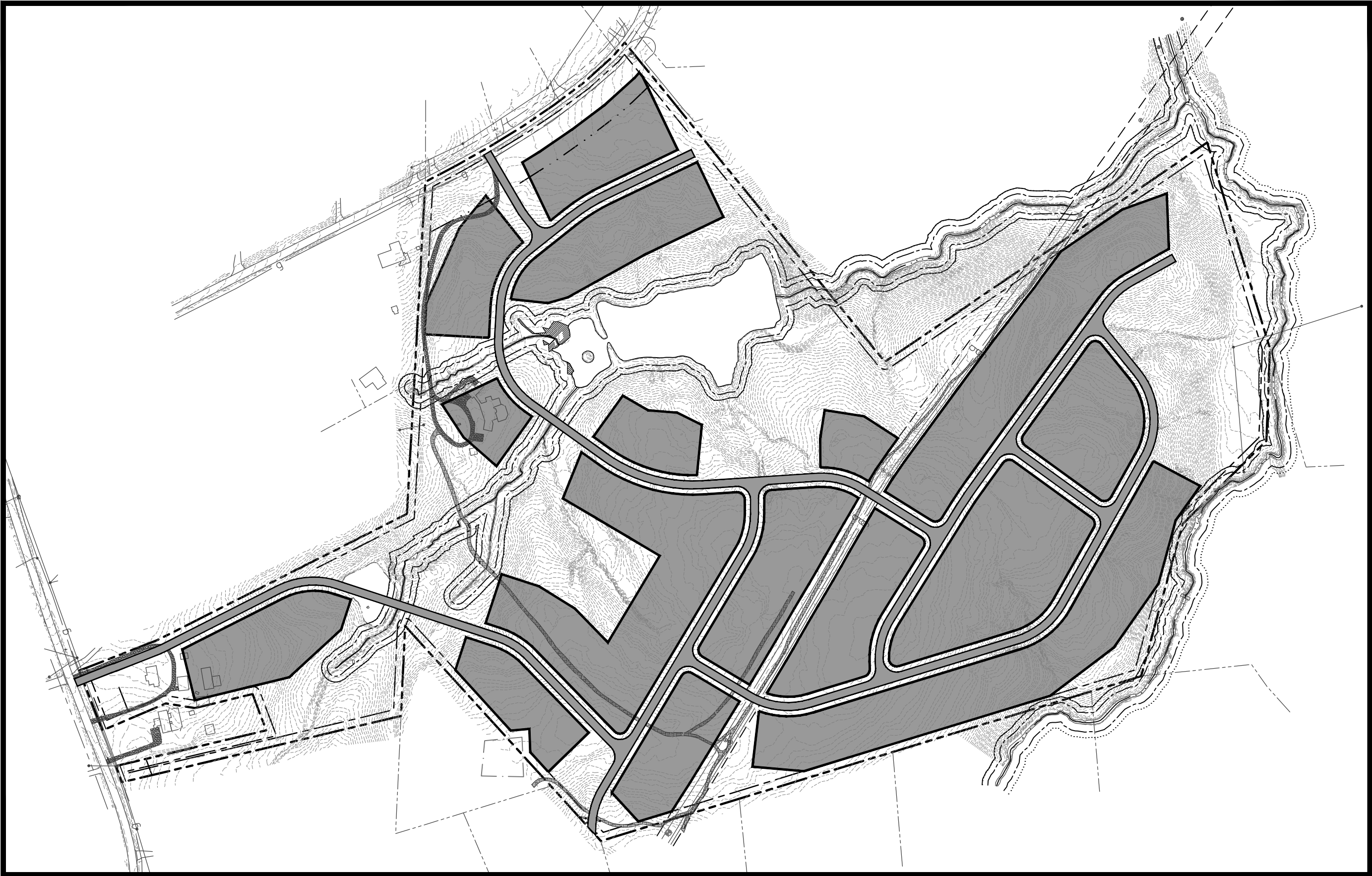


STEP 1: IDENTIFY OPEN SPACE

AREA 1: THIS AREA IS OPEN SPACE BY DEFAULT, AND WILL BE CLASSIFIED AS "NATURAL OPEN SPACE". IT IS COMPRISED OF TWO 80'-WIDE OPAQUE PERIMETER BUFFERS, WHICH ARE REQUIRED WHERE THE PROPERTY LINE ADJACENTS A STATE-MAINTAINED ROAD, AND A 20'-WIDE BUFFER ALONG ALL OTHER SECTIONS OF THE PROPERTY LINE. THE ONLY SECTION OF THE PROPERTY LINE, WHICH DOES NOT HAVE A BUFFER ASSOCIATED WITH IT, IS THE SEGMENT LOCATED JUST NORTH OF THE MAIN WESTERN ENTRANCE. THE BUFFER REQUIREMENT HAS BEEN WAIVED HERE TO ALLOW FOR THE ENTRANCE ROAD TO BE PLACED AS FAR NORTH AS POSSIBLE, ALIGNED WITH THE INTERSECTING STREET ACCESSING THE FARMHOUSE CLUSTER.

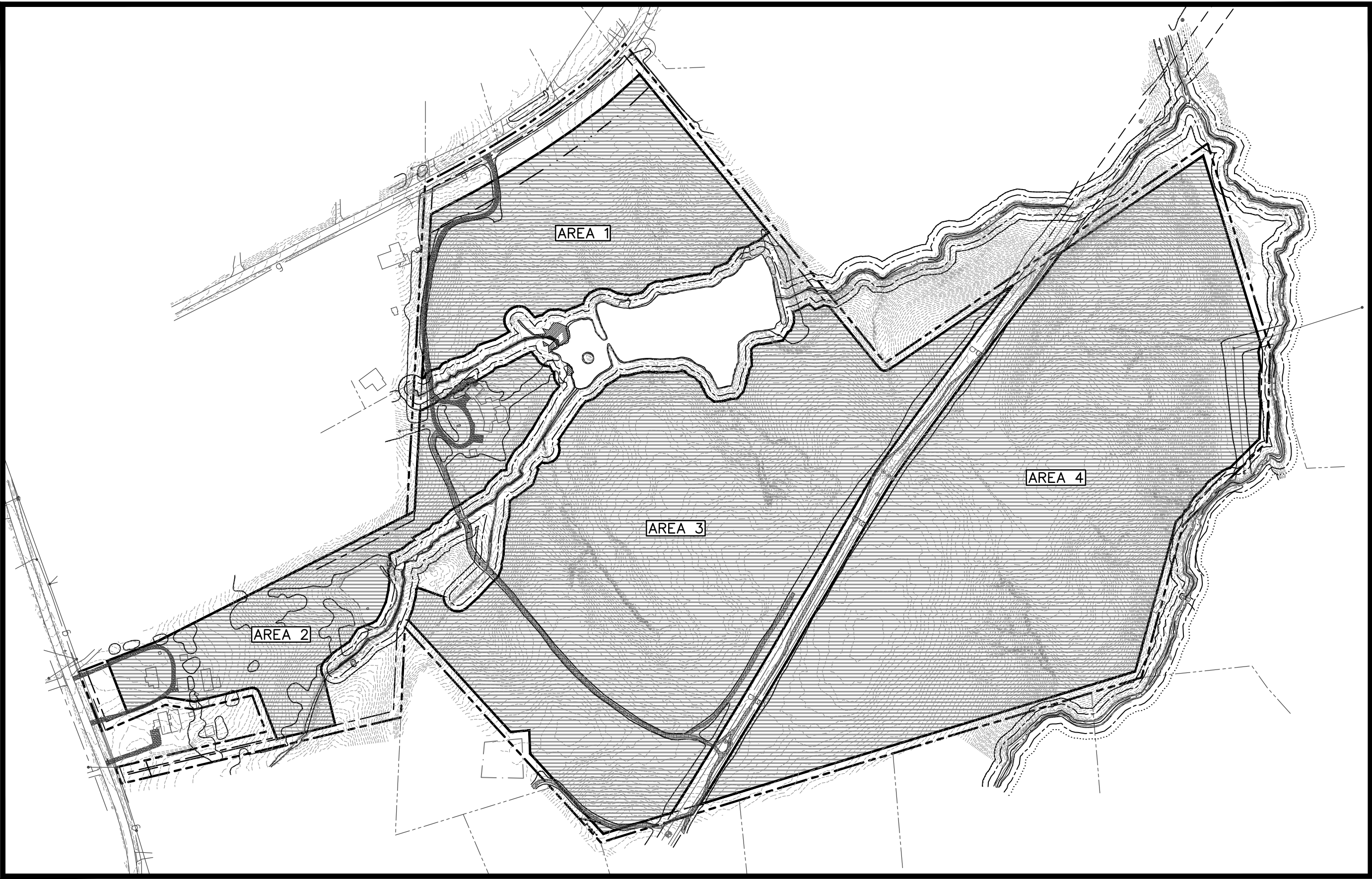
AREA 2: THIS AREA IS OPEN SPACE BY DEFAULT, AND WILL BE CLASSIFIED AS "NATURAL OPEN SPACE". THIS AREA FOLLOWS THE COURSE OF THE JURISDICTIONAL PERENNIAL AND INTERMITTENT STREAMS WHICH FLOW THROUGH THE SITE, AND INCLUDES THE 35'-WIDE STREAM BUFFER WHICH IS REQUIRED ON EACH SIDE OF THE STREAM, AS MEASURED LANDWARD FROM THE TOP OF BANK.

AREA 3: THIS AREA IS OPEN SPACE BY DEFAULT. IT IS MADE UP ENTIRELY OF THE EXISTING 50' WIDE GAS RIGHT-OF-WAY THAT RUNS DIAGONALLY THROUGH THE SITE. THE MAJORITY OF THIS AREA WILL BE CLASSIFIED AS "NATURAL OPEN SPACE," BUT A SMALL SECTION, WHERE AN EXISTING UNPAVED ROAD WILL BE CONVERTED INTO A MAINTAINED NATURE TRAIL WILL BE CLASSIFIED AS URBAN OPEN SPACE.



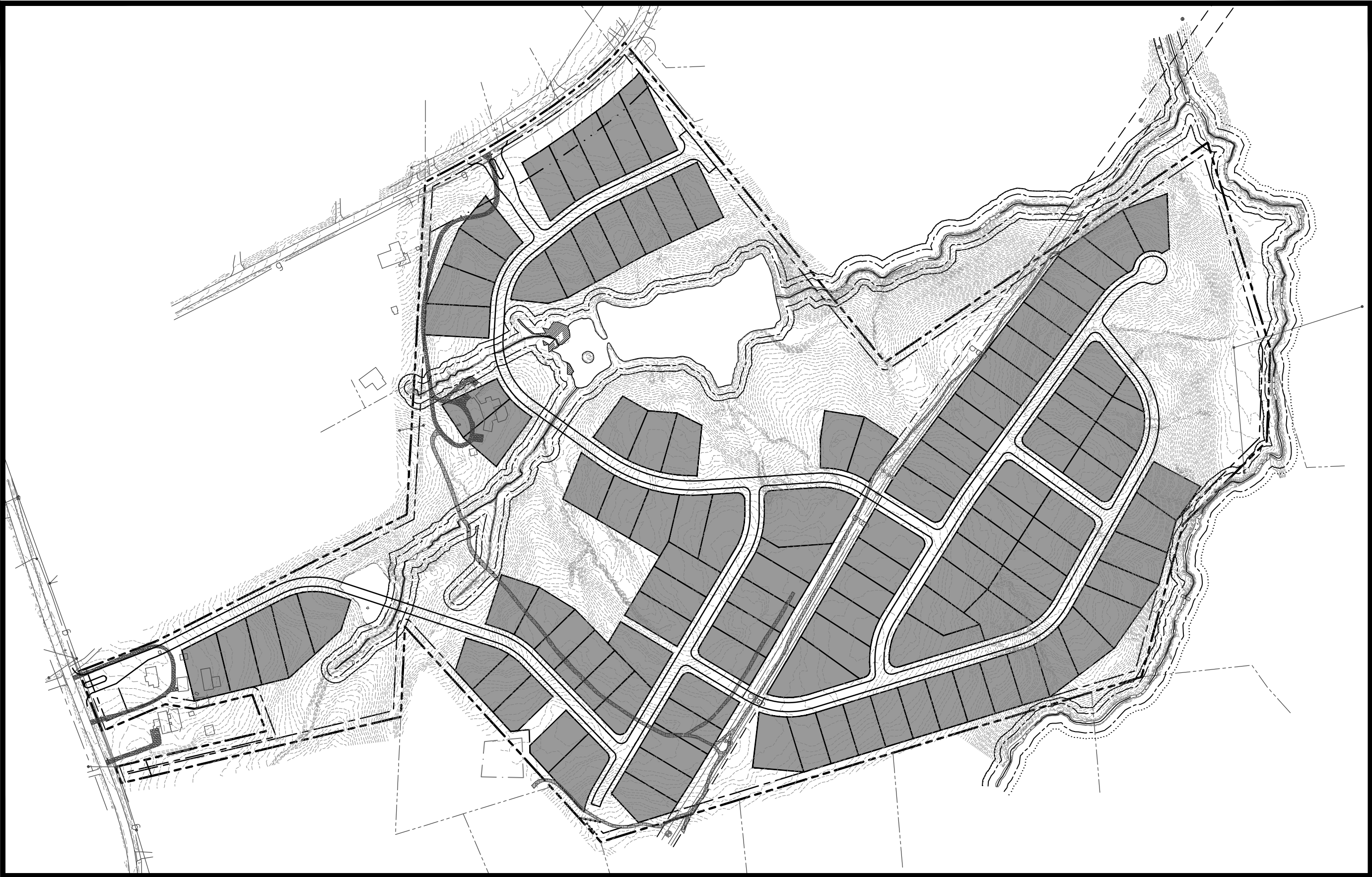
STEP 3: STREET AND LOT LAYOUT

TO RELIEVE TRAFFIC CONGESTION ISSUES AND MAXIMIZE THE OVERALL ACCESSIBILITY OF THE DEVELOPMENT, IT WAS DECIDED THAT TWO ENTRANCES WOULD BE CONSTRUCTED - ONE AT THE NORTHERN CONNECTION TO ASBURY CHAPEL ROAD, AND A SECOND AT A WESTERN CONNECTION POINT. THESE ENTRANCES ARE DESIGNED TO LEAVE A LASTING POSITIVE IMPRESSION ON ALL THOSE ENTERING THE SITE BY UTILIZING LANDSCAPED OPEN SPACES AT THEIR CONNECTION POINTS, PROVIDING WELL-PRESERVED STREAM CROSSINGS, AND MAINTAINING A DIVERSE ARRAY OF SPECIMEN TREES THAT CAN BE VIEWED FROM THE ROAD. ONCE THE LOCATION OF THESE ENTRANCES WAS SET, THE ROADS WERE LAID OUT IN SUCH A WAY AS TO CONNECT ALL OF THE HOUSING AREAS DESCRIBED ABOVE IN STEP 2, AVOIDING STEEPLY-SLOPED SECTIONS OF THE PROPERTY WHILE ALSO STRIVING TO PRESERVE IMPORTANT NATURAL FEATURES AND MAXIMIZE THE OVERALL AESTHETIC APPEAL OF THE DEVELOPMENT. THIS WAS ACCOMPLISHED IN A NUMBER OF WAYS. FIRST, ALL ROADWAYS INTERSECT WITH STREAMS AT AS NEAR TO 90° AS POSSIBLE TO MINIMIZE DISTURBANCE. THE ROADWAY INTERSECTING THE GAS LINE RIGHT OF WAY WAS ALSO REQUIRED TO CROSS PERPENDICULARLY TO MINIMIZE DISTURBANCE THERE. ALTHOUGH TECHNICALLY REQUIRED, THE MINIMIZATION OF THESE DISTURBANCES GOES ALONG WAY TOWARDS PRESERVING THE NATURAL BEAUTY OF THE SITE. IN ADDITION TO THIS, EFFORTS WERE MADE TO LOCATE THE ROADS ON THE PERIPHERY OF AREAS OF GREAT NATURAL AESTHETIC VALUE, SUCH AS THE HIGHLY VEGETATED AREAS NEAR THE STREAMS AND, IN SOME CASES, BETWEEN LOTS. THESE AREAS WERE CHOSEN FOR THEIR ABUNDANCE OF MATURE HARDWOOD AND SOFTWOOD TREE SPECIES, INCLUDING A LARGE CONCENTRATION OF SPECIMEN TREES AT THE WESTERN ENTRANCE. AN EFFORT WAS MADE TO LAYOUT ROADS AT THE PERIPHERY OF THESE HIGH-VALUE AREAS TO PRESERVE THE NATURAL BEAUTY OF THE SITE, WHILE SIMULTANEOUSLY PROVIDING SCENIC VIEWS TO ALL RESIDENTS AND VISITORS.



STEP 2: LOCATION OF HOUSE SITES

THE FOUR AREAS IDENTIFIED FOR HOUSING SITES ARE LOCATED ON THE PROPERTY IN THE AREAS WHICH ARE NOT ALREADY DESIGNATED FOR NON-BUILDABLE FEATURES, SUCH AS BUFFERS, EASEMENTS, AND STREAMS. AN EFFORT HAS BEEN MADE TO POSITION THE HOUSING SITES AWAY FROM HIGHLY-SLOPED AREAS AND SECTIONS OF THE PROPERTY IN WHICH ACCESSIBILITY WOULD BE AN ISSUE, SUCH AS SMALL, IRREGULAR AREAS BETWEEN STREAM BUFFERS AND THE RELATIVELY THIN SWATH OF LAND AT THE WESTERN EDGE OF THE PROPERTY. AS SUCH, THESE AREAS REPRESENT ONLY THE OUTERMOST LIMITS OF WHERE LOTS CAN BE PLACED; A MORE DETAILED ANALYSIS REGARDING HOUSING PLACEMENT WAS CONDUCTED LATER IN THE PROCESS, IN WHICH OTHER CONSIDERATIONS SUCH AS TREE PRESERVATION, SCENIC VIEWS, AND AESTHETIC APPEAL WERE ACCOUNTED FOR.



STEP 4: DRAW THE LOT LINES

HAVING PREVIOUSLY DEFINED THE LOCATION OF ROADWAYS AND BUILDABLE AREAS, THE FINAL STEP IN THE PROCESS WAS TO DRAW THE LOT LINES IN THE SECTIONS DESIGNATED FOR HOUSING SITES. IN DOING THIS, AN EFFORT WAS MADE TO PROVIDE A BALANCE OF LOT SIZES, RANGING IN SIZE FROM ROUGHLY 10,000 SF TO 15,000 SF, WHILE OBSERVING ALL AVERAGE SETBACK AND LOT SIZE REQUIREMENTS. GENERALLY SPEAKING, THE SITE IS COMPRISED OF DIFFERENTIATED CLUSTERS OF LARGE, MEDIUM, AND SMALL LOTS TO PREVENT THE LARGEST LOTS FROM EXERTING A DWARFING EFFECT ON THE SMALLEST LOTS, AND VICE-VERSA. TO CREATE SOME VARIETY THROUGHOUT THE DEVELOPMENT AND AVOID UNINTERESTING STRAIGHT STRETCHES OF HOUSE LINES, LOTS WERE LOCATED AROUND ROADWAY CURVES WHENEVER POSSIBLE, AND THEIR SHAPES WERE VARIED AS MUCH AS POSSIBLE, WHILE STILL ACHIEVING THE DESIRED DENSITY.

Ryan
Homes

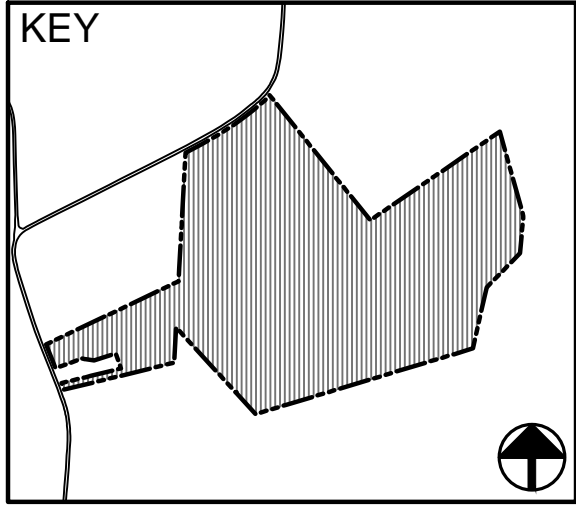
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ISSUANCE INDEX		
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PROJECT PHASE:	ALL	

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NO.	DESCRIPTION	DATE
A	PRELIM. SKETCH PLAN 1	4/1/2016
B	PRELIM. SKETCH PLAN 2	5/18/2016

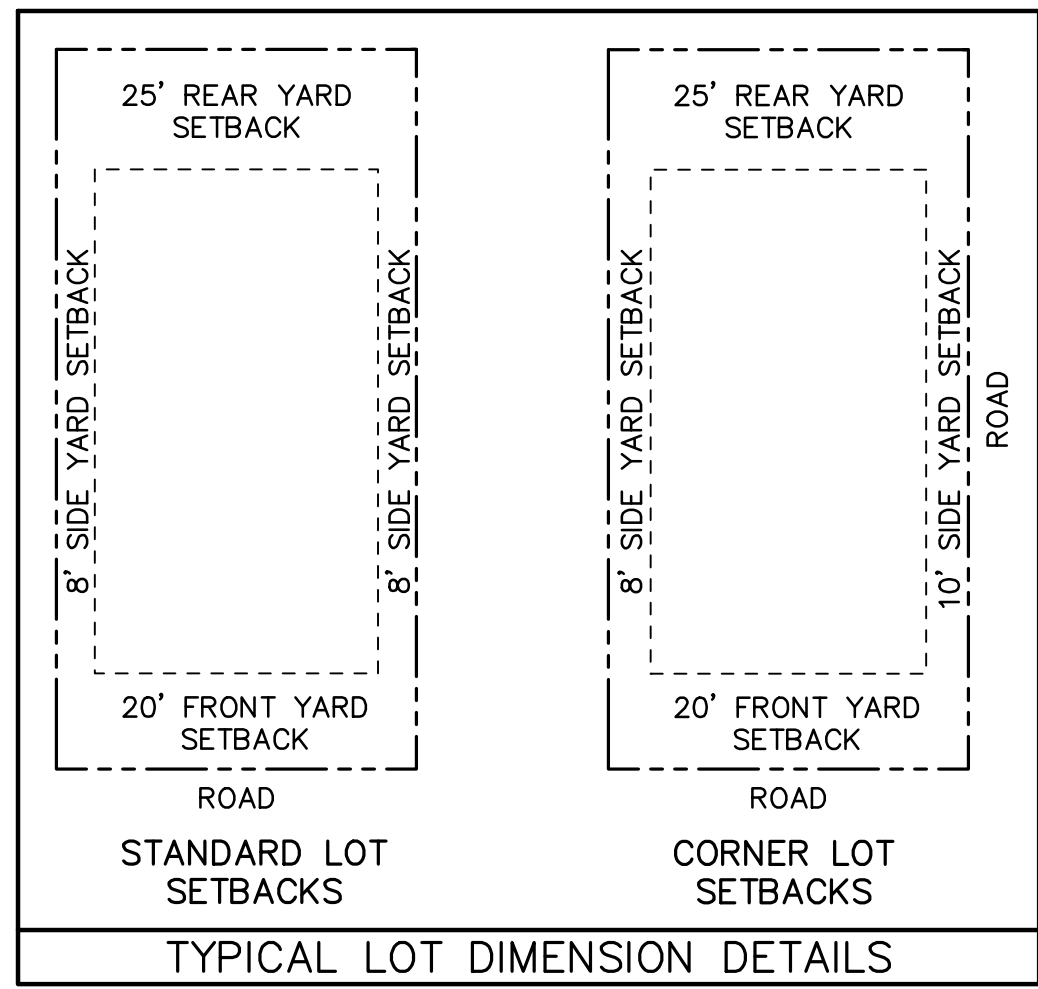
STRUCTUREPOINT PROJECT # 2015.00721

FOUR STEP
PROCESS

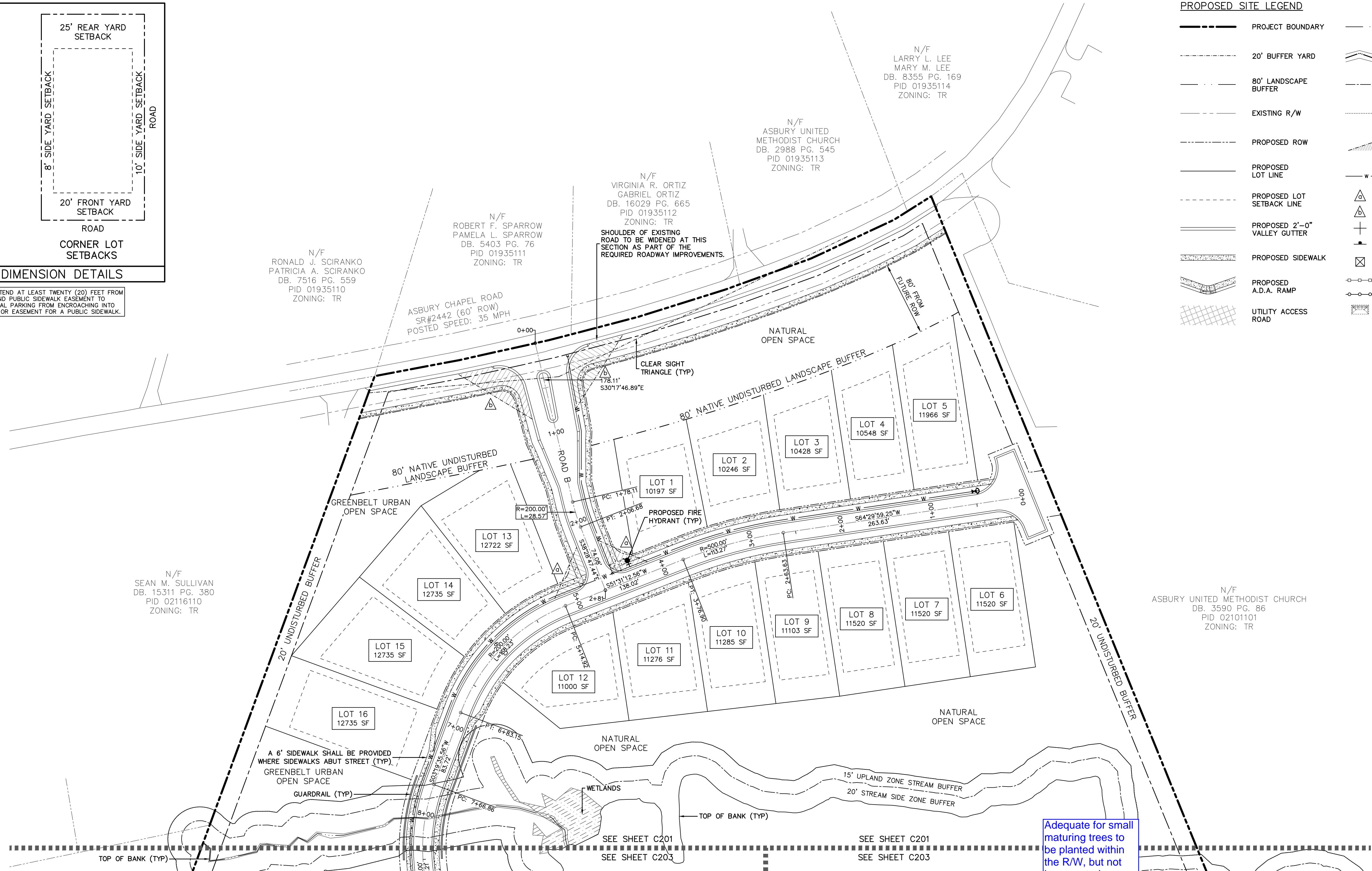
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PRINT DATE: 5/19/16 PLOT SCALE: 1"=60'



NOTE: DRIVEWAYS SHALL EXTEND AT LEAST TWENTY (20) FEET FROM A PUBLIC RIGHT-OF-WAY AND PUBLIC SIDEWALK EASEMENT TO PREVENT ON-SITE RESIDENTIAL PARKING FROM ENCRDACHING INTO THE PUBLIC RIGHT-OF-WAY OR EASEMENT FOR A PUBLIC SIDEWALK.



PROPOSED SITE LEGEND

- PROJECT BOUNDARY
- 20' BUFFER YARD
- 80' LANDSCAPE BUFFER
- EXISTING R/W
- PROPOSED ROW
- PROPOSED LOT LINE
- PROPOSED LOT SETBACK LINE
- PROPOSED 2'-0" VALLEY GUTTER
- PROPOSED SIDEWALK
- PROPOSED A.D.A. RAMP
- UTILITY ACCESS ROAD
- POND BOUNDARY (NORMAL POOL)
- PERENNIAL STREAM CENTERLINE & TOP OF BANK
- 20' STREAM SIDE ZONE STREAM BUFFER (NO DISTURB)
- 15' UPLAND ZONE STREAM BUFFER
- SIGHT TRIANGLE
- PROPOSED WATER LINE & FIRE HYDRANT
- 15'X15' SIGHT TRIANGLE
- 35'X35' SIGHT TRIANGLE
- PROPOSED STREET SIGN
- PROPOSED STOP SIGN STANDARD MUTCD (R1-1)
- PROPOSED NO PARKING SIGN
- SPLIT-RAIL FENCE
- VEHICLE BARRICADE
- MAILBOX
- USFS CLUSTER BOX UNIT

Ryan Homes

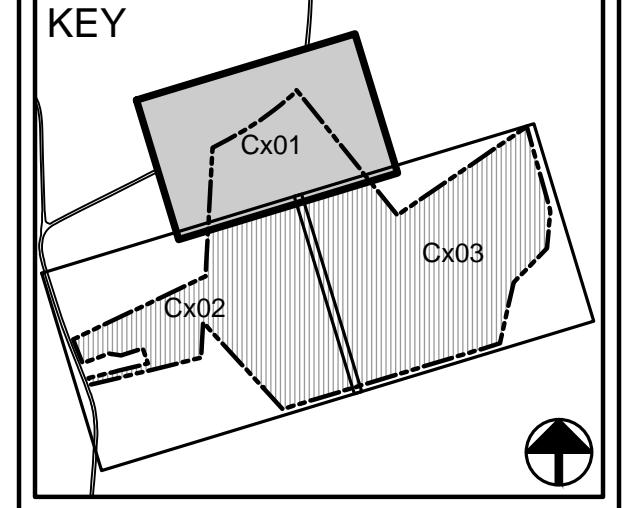
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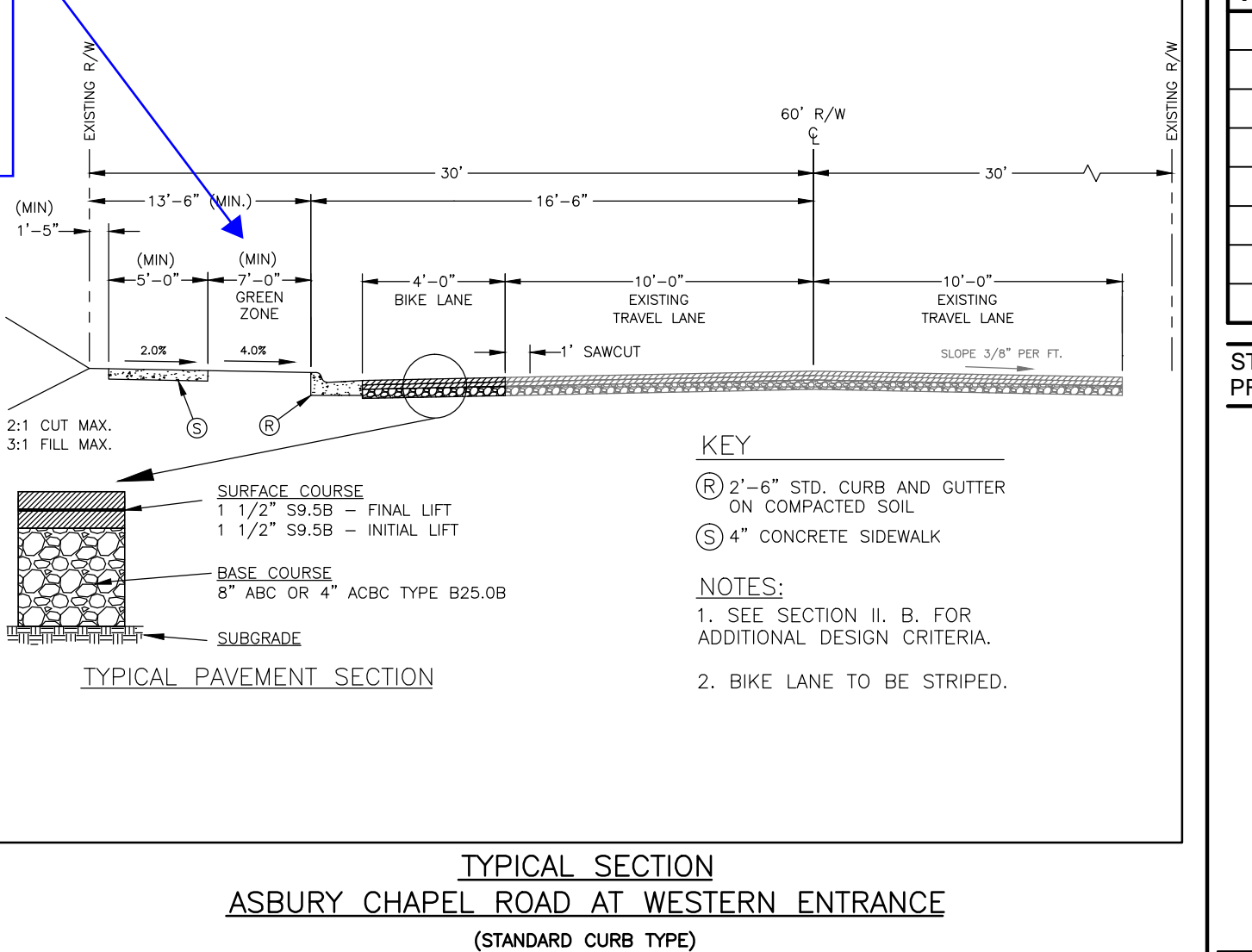
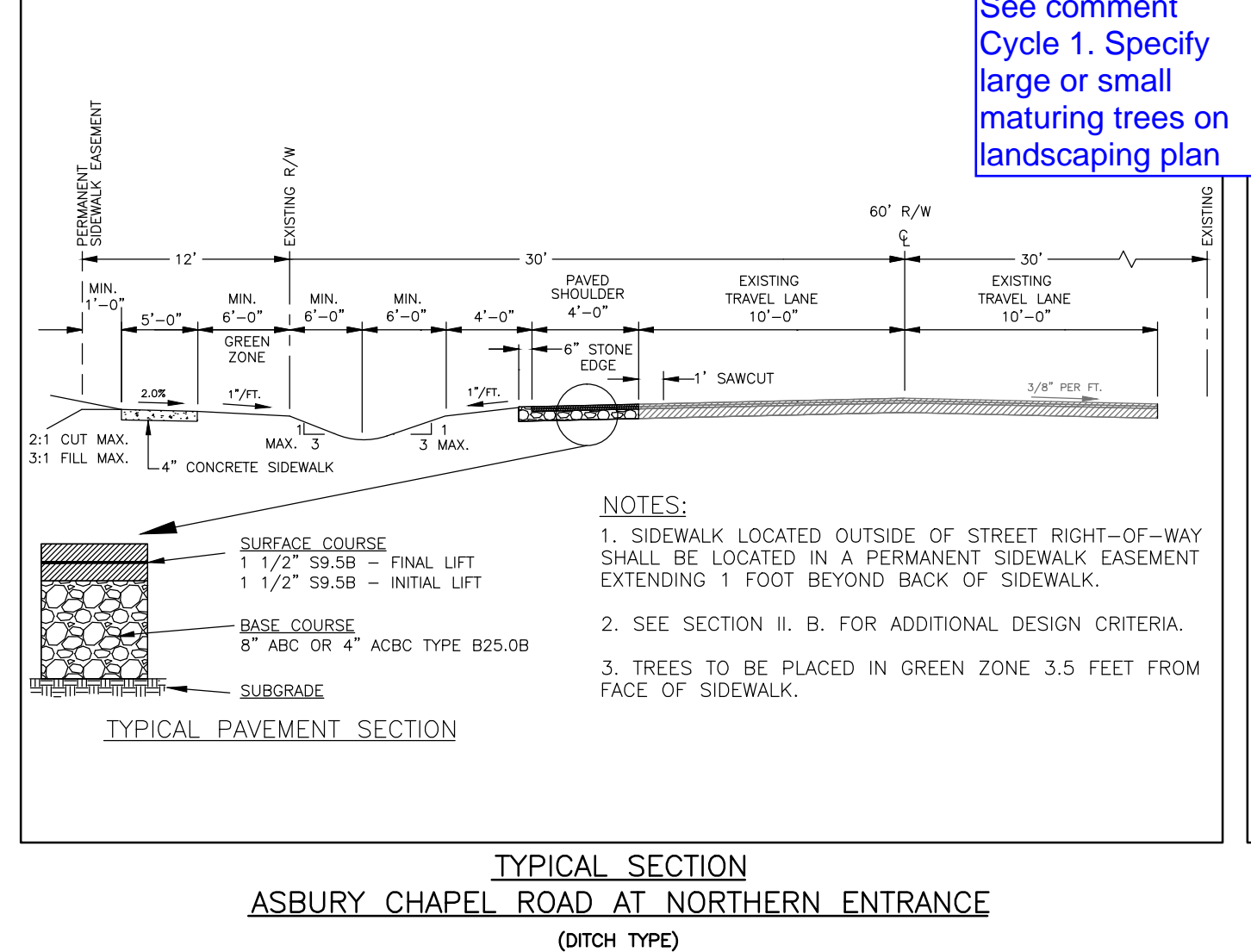
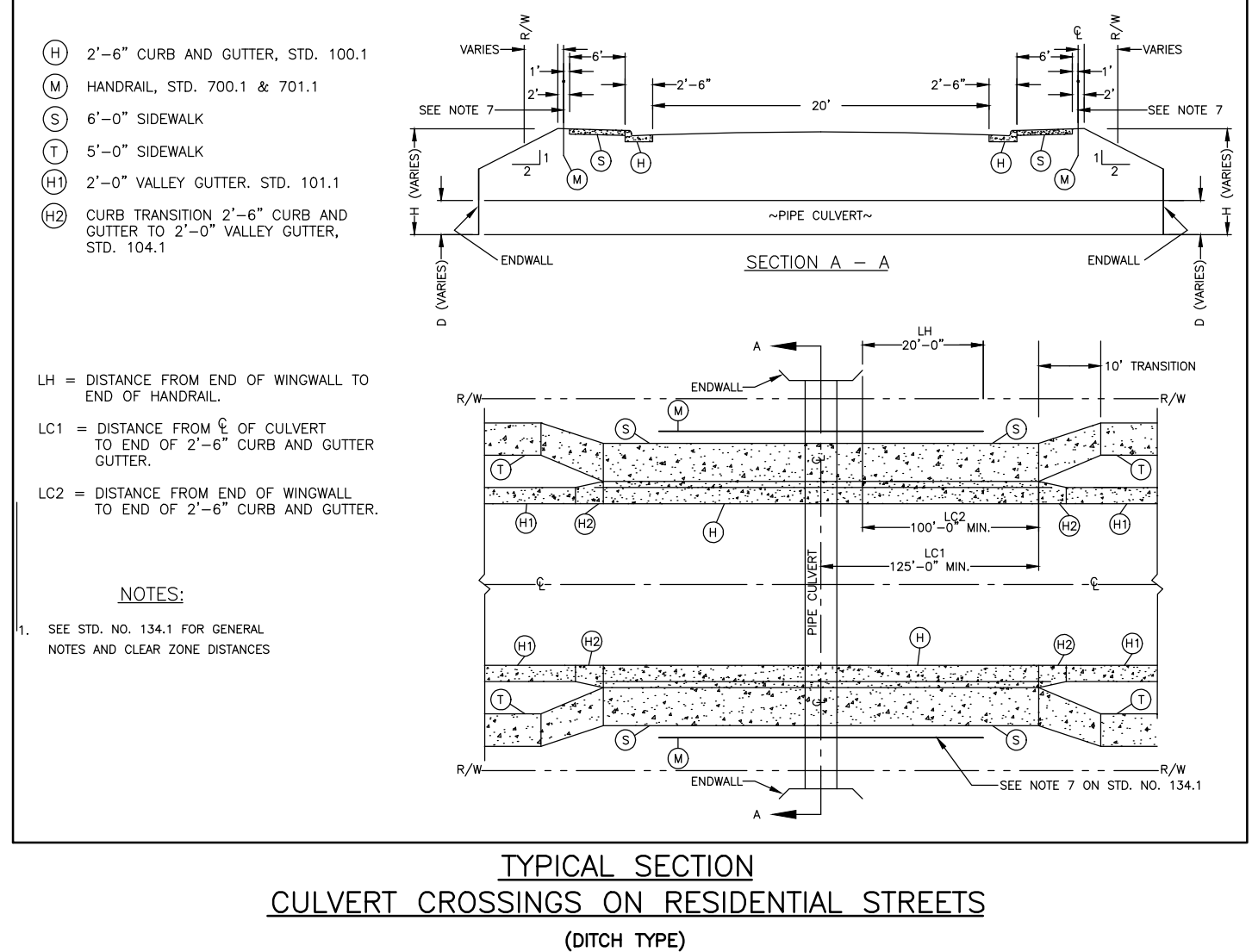
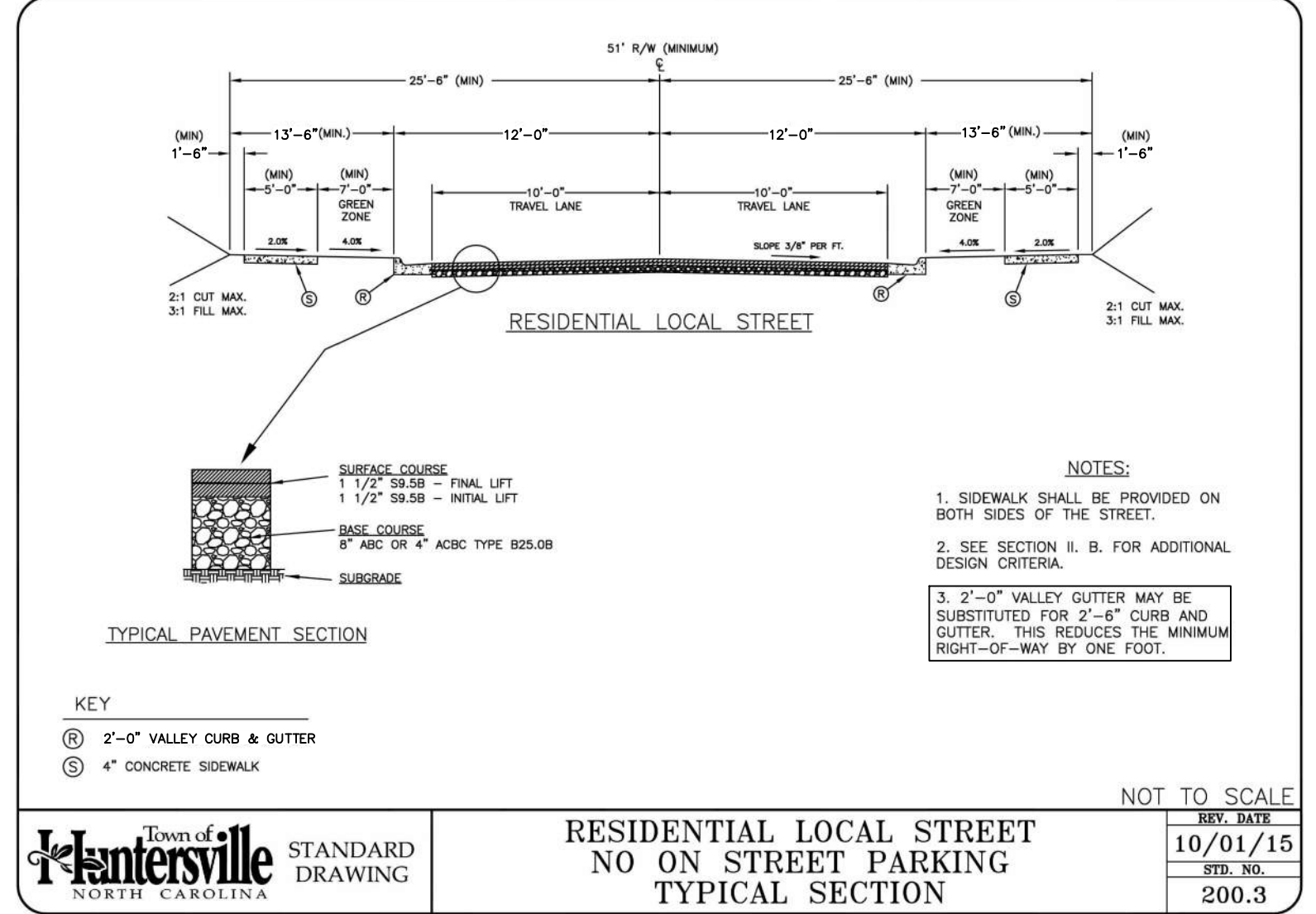
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SITE PLAN

C201

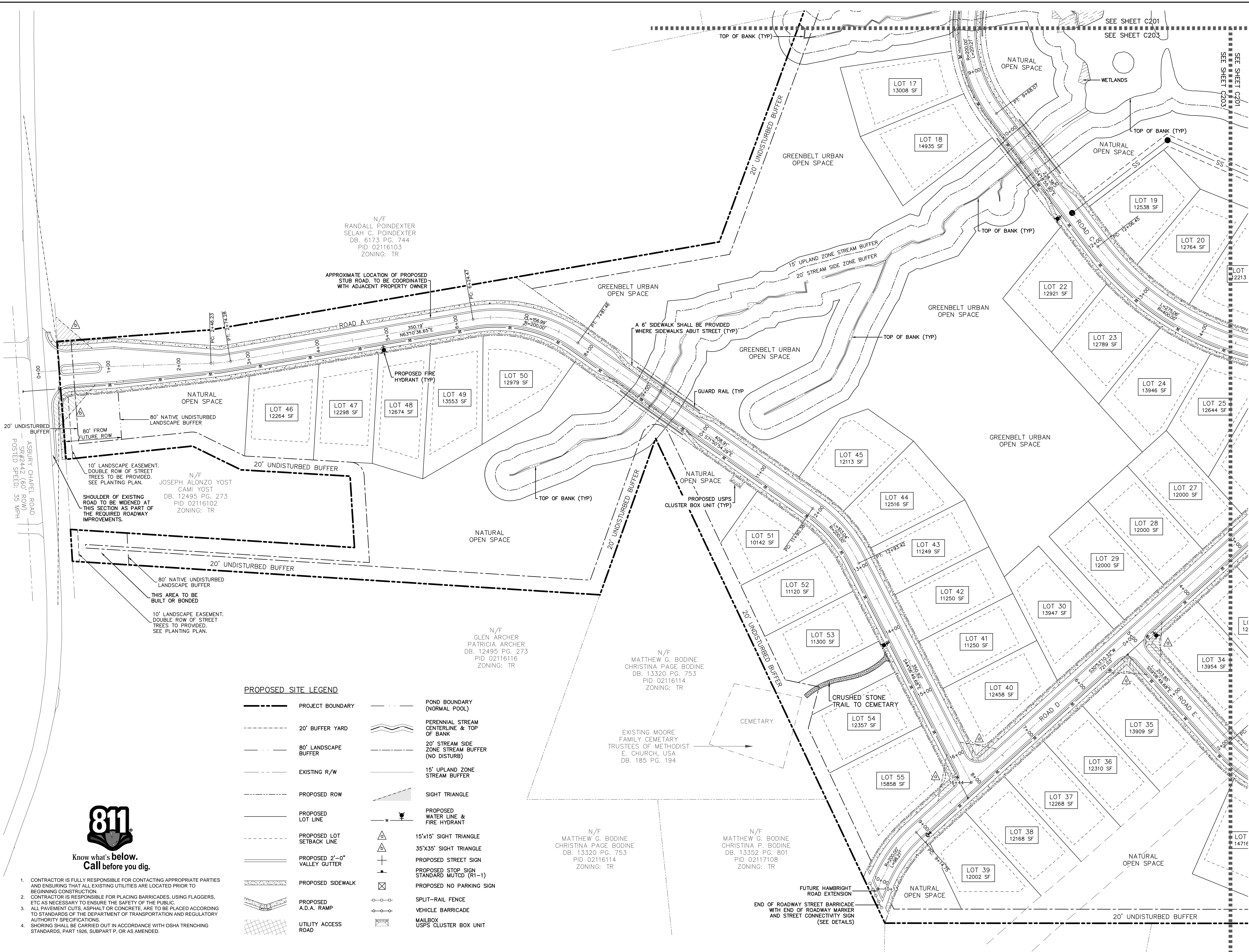
0' 60' 120'

SCALE: 1"=60'



Adequate for small maturing trees to be planted within the R/W, but not large maturing. See comment Cycle 1. Specify large or small maturing trees on landscaping plan

PRINT DATE: 5/19/16 PLOT SCALE: 1"=60' EDIT DATE: 5/19/16 - 12:56 PM EDITED BY: DOHAUSSARD DRAWING FILE: P:\2015\00721\0. DRAWINGS\01\CONSTRUCTION DOCUMENTS\2015.00721.00.C200.SITE_PLAN.DWG

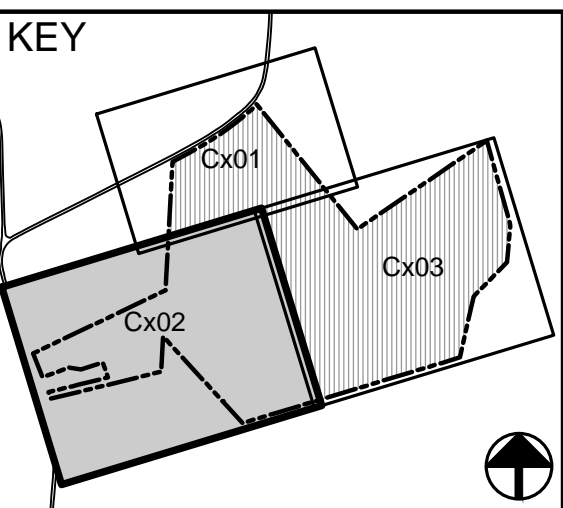


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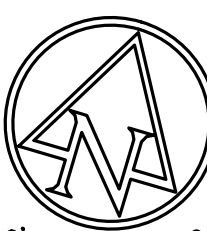
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SITE PLAN

C202



0' 60' 120'
SCALE: 1"=60'