

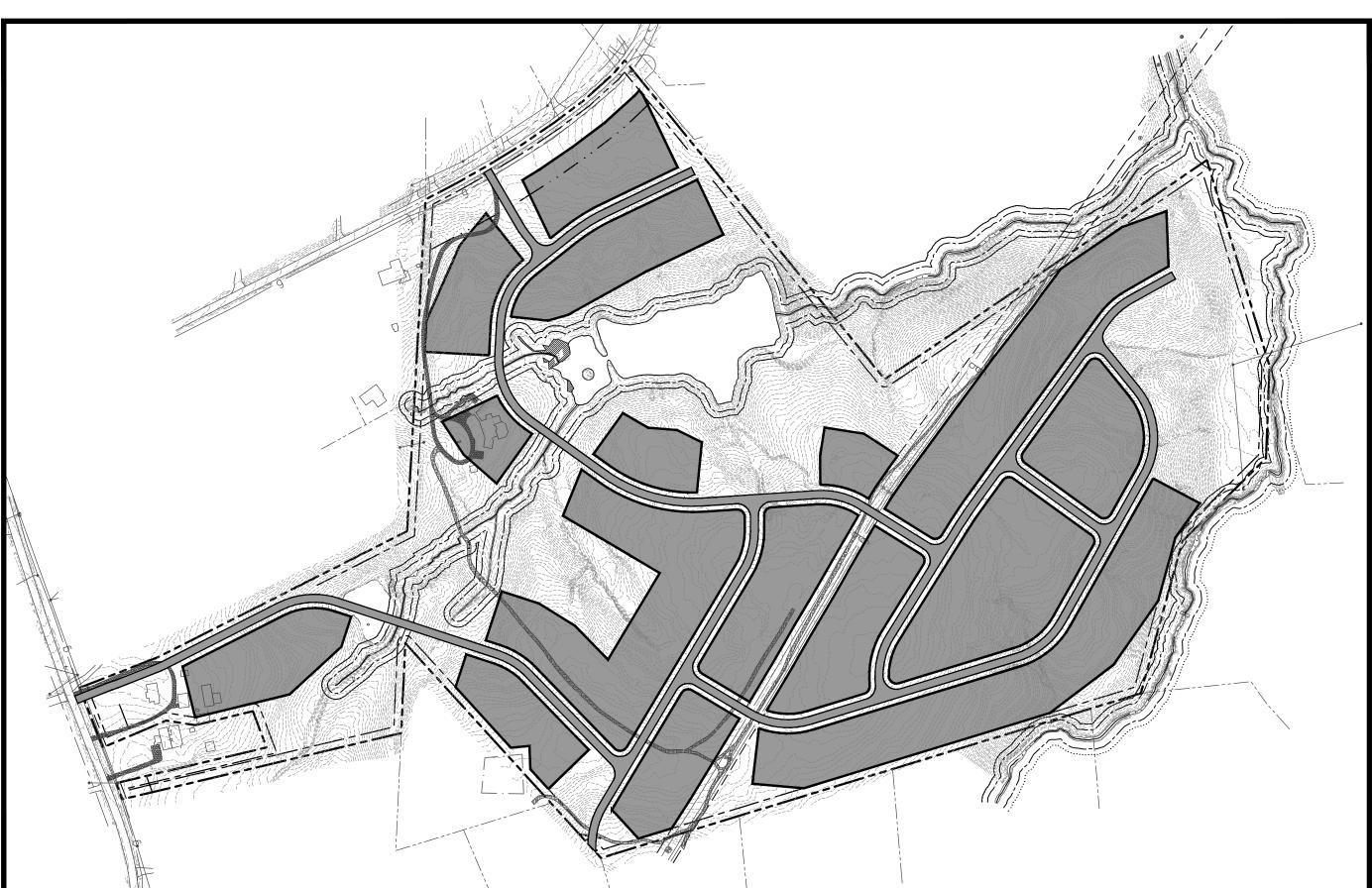
STEP 1: IDENTIFY OPEN SPACE

AREA 1: THIS AREA IS OPEN SPACE BY DEFAULT, AND WILL BE CLASSIFIED AS "NATURAL OPEN SPACE". IT IS COMPRISED OF TWO 80'-WIDE OPAQUE PERIMETER BUFFERS, WHICH ARE REQUIRED WHERE THE PROPERTY LINE ABUTS A STATE-MAINTAINED ROAD, AND A 20'-WIDE BUFFER ALONG ALL OTHER SECTIONS OF THE PROPERTY LINE. THE ONLY SECTION OF THE PROPERTY LINE, WHICH DOES NOT HAVE A BUFFER ASSOCIATED WITH IT, IS THE SEGMENT LOCATED JUST NORTH OF THE MAIN WESTERN ENTRANCE. THE BUFFER REQUIREMENT HAS BEEN WAIVED HERE TO ALLOW FOR THE ENTRANCE ROAD TO BE PLACED AS FAR NORTH AS POSSIBLE, ALIGNED WITH THE INTERSECTING STREET ACCESSING THE FARMHOUSE CLUSTER.

AREA 2: THIS AREA IS OPEN SPACE BY DEFAULT, AND WILL BE CLASSIFIED AS "NATURAL OPEN SPACE". THIS AREA FOLLOWS THE COURSE OF THE JURISDICTIONAL PERENNIAL AND INTERMITTENT STREAMS WHICH FLOW THROUGH THE SITE, AND INCLUDES THE 35'-WIDE STREAM BUFFER WHICH IS REQUIRED ON EACH SIDE OF THE STREAM, AS MEASURED LANDWARD FROM THE TOP OF BANK.

AREA 3: THIS AREA IS OPEN SPACE BY DEFAULT. IT IS MADE UP ENTIRELY OF THE EXISTING 50' WIDE GAS RIGHT-OF-WAY THAT RUNS DIAGONALLY THROUGH THE SITE. THE MAJORITY OF THIS AREA WILL BE CLASSIFIED AS

"NATURAL OPEN SPACE," BUT A SMALL SECTION, WHERE AN EXISTING UNPAVED ROAD WILL BE CONVERTED INTO A MAINTAINED NATURE TRAIL WILL BE CLASSIFIED AS URBAN OPEN SPACE.

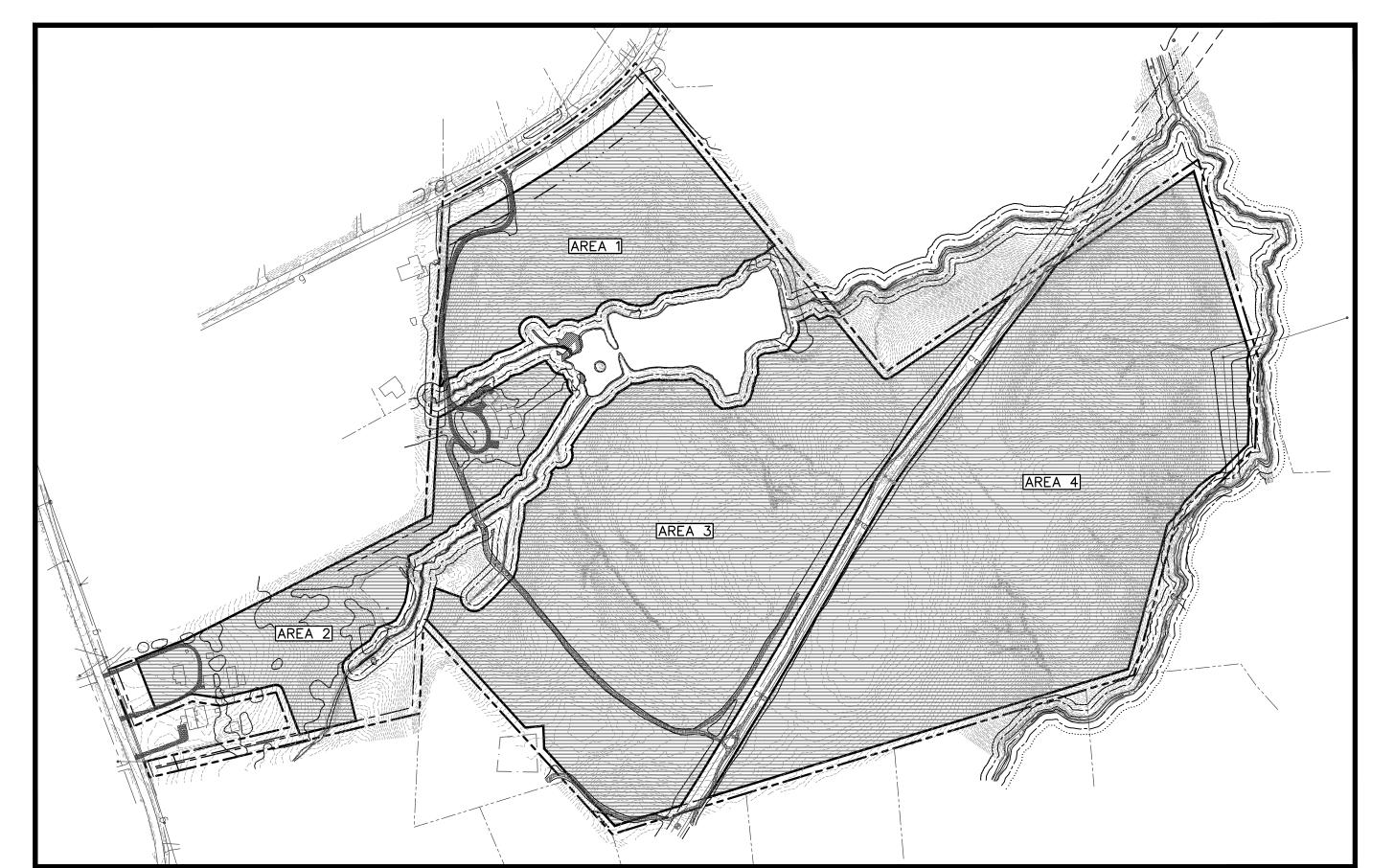


STEP 3: STREET AND LOT LAYOUT

TO RELIEVE TRAFFIC CONGESTION ISSUES AND MAXIMIZE THE OVERALL ACCESSIBILITY OF THE DEVELOPMENT, IT WAS DECIDED THAT TWO ENTRANCES WOULD BE CONSTRUCTED — ONE AT THE NORTHERN CONNECTION TO ASBURY CHAPEL ROAD, AND A SECOND AT A WESTERN CONNECTION POINT. THESE ENTRANCE ARE DESIGNED TO LEAVE A LASTING POSITIVE IMPRESSION ON ALL THOSE ENTERING THE SITE BY UTILIZING LANDSCAPED OPEN SPACES AT THEIR CONNECTION POINTS, PROVIDING WELL—PRESERVED STREAM CROSSINGS, AND MAINTAINING A DIVERSE ARRAY OF SPECIMEN TREES THAT CAN BE VIEWED FROM THE ROAD.

ONCE THE LOCATION OF THESE ENTRANCES WAS SET, THE ROADS WERE LAID OUT IN SUCH A WAY AS TO CONNECT ALL OF THE HOUSING AREAS DESCRIBED ABOVE IN STEP 2, AVOIDING STEEPLY—SLOPED SECTIONS OF THE PROPERTY WHILE ALSO STRIVING TO PRESERVE IMPORTANT NATURAL FEATURES AND MAXIMIZE THE OVERALL AESTHETIC APPEAL OF THE DEVELOPMENT. THIS WAS ACCOMPLISHED IN A NUMBER OF WAYS. FIRST, ALL ROADWAYS INTERSECT WITH STREAMS AT AS NEAR TO 90° AS POSSIBLE TO MINIMIZE DISTURBANCE. THE ROADWAY INTERSECTING THE GAS LINE RIGHT OF WAY WAS ALSO REQUIRED TO CROSS PERPENDICULARLY TO MINIMIZE DISTURBANCE THERE. ALTHOUGH TECHNICALLY REQUIRED, THE MINIMIZATION OF THESE DISTURBANCES GOES ALONG WAY TOWARDS PRESERVING THE NATURAL BEAUTY OF THE SITE.

IN ADDITION TO THIS, EFFORTS WERE MADE TO LOCATE THE ROADS ON THE PERIPHERY OF AREAS OF GREAT NATURAL AESTHETIC VALUE, SUCH AS THE HIGHLY VEGETATED AREAS NEAR THE STREAMS AND, IN SOME CASES, BETWEEN LOTS. THESE AREAS WERE CHOSEN FOR THEIR ABUNDANCE OF MATURE HARDWOOD AND SOFTWOOD TREE SPECIES, INCLUDING A LARGE CONCENTRATION OF SPECIMEN TREES AT THE WESTERN ENTRANCE. AN EFFORT WAS MADE TO LAYOUT ROADS AT THE PERIPHERY OF THESE HIGH-VALUE AREAS TO PRESERVE THE NATURAL BEAUTY OF THE SITE, WHILE SIMULTANEOUSLY PROVIDING SCENIC VIEWS TO ALL RESIDENTS AND VISITORS.



STEP 2: LOCATION OF HOUSE SITES

THE FOUR AREAS IDENTIFIED FOR HOUSING SITES ARE LOCATED ON THE PROPERTY IN THE AREAS WHICH ARE NOT ALREADY DESIGNATED FOR NON-BUILDABLE FEATURES, SUCH AS BUFFERS, EASEMENTS, AND STREAMS. AN EFFORT HAS BEEN MADE TO POSITION THE HOUSING SITES AWAY FROM HIGHLY-SLOPED AREAS AND SECTIONS OF THE PROPERTY IN WHICH ACCESSIBILITY WOULD BE AN ISSUE, SUCH AS SMALL IRREGULAR AREAS BETWEEN STREAM BUFFERS AND THE RELATIVELY THIN SWATH OF LAND AT THE WESTERN EDGE OF THE PROPERTY. AS SUCH, THESE AREA REPRESENT ONLY THE OUTERMOST LIMITS OF WHERE LOTS CAN BE PLACED; A MORE DETAILED ANALYSIS REGARDING HOUSING PLACEMENT WAS CONDUCTED LATER IN THE PROCESS, IN WHICH OTHER CONSIDERATIONS SUCH AS TREE PRESERVATION, SCENIC VIEWS, AND AESTHETIC APPEAL WERE ACCOUNTED FOR.



STEP 4: DRAW THE LOT LINES

HAVING PREVIOUSLY DEFINED THE LOCATION OF ROADWAYS AND BUILDABLE AREAS, THE FINAL STEP IN THE PROCESS WAS TO DRAW THE LOT LINES IN THE SECTIONS DESIGNATED FOR HOUSING SITES. IN DOING THIS, AN EFFORT WAS MADE TO PROVIDE A BALANCE OF LOT SIZES, RANGING IN SIZE FROM ROUGHLY 10,000 SF TO 15,000 SF, WHILE OBSERVING ALL AVERAGE SETBACK AND LOT SIZE REQUIREMENTS. GENERALLY SPEAKING, THE SITE IS COMPRISED OF DIFFERENTIATED CLUSTERS OF LARGE, MEDIUM, AND SMALL LOTS TO PREVENT THE LARGEST LOTS FROM EXERTING A DWARFING EFFECT ON THE SMALLEST LOTS, AND VICE—VERSA. TO CREATE SOME VARIETY THROUGHOUT THE DEVELOPMENT AND AVOID UNINTERESTING STRAIGHT STRETCHES OF HOUSE LINES, LOTS WERE LOCATED AROUND ROADWAY CURVES WHENEVER POSSIBLE, AND THEIR SHAPES WERE VARIED AS MUCH AS POSSIBLE, WHILE STILL ACHIEVING THE DESIRED DENSITY.

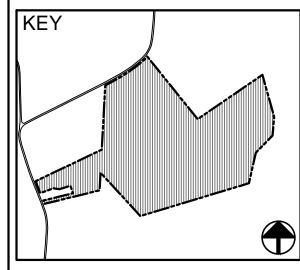


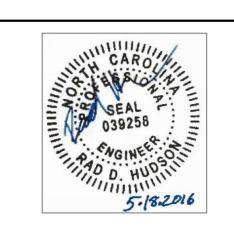


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ASBURY CHAPEL

11620 Asbury Chapel Road Huntersville, NC 28078





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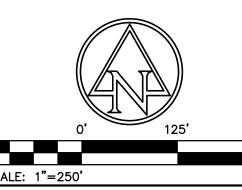
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В	PRELIM. SKETCH PLAN 2	5/18/2016

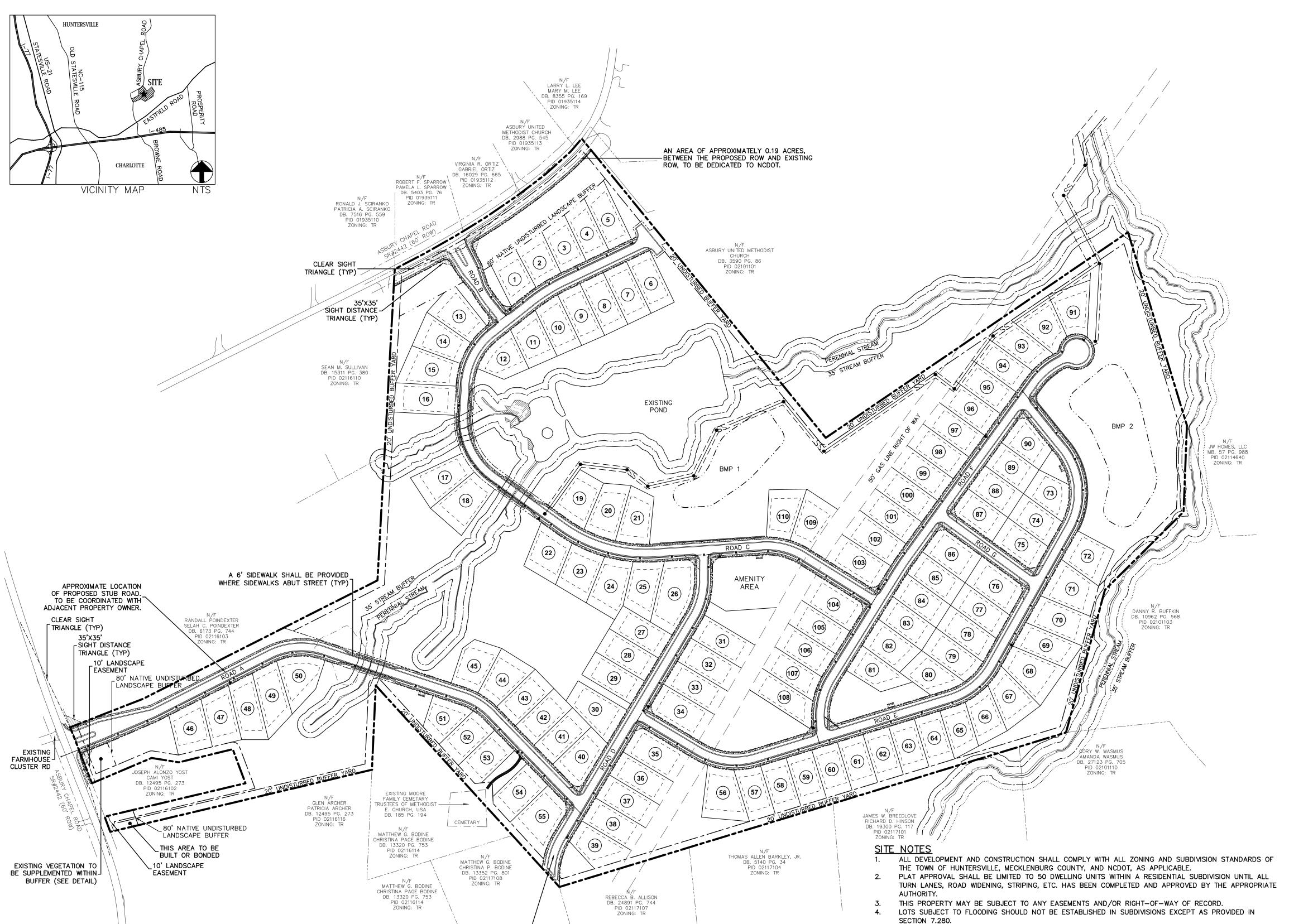
STRUCTUREPOINT PROJECT # 2015.00721

FOUR STEP PROCESS

C15



PRINT DATE: 5/19/16



STUB FOR FUTURE HAMBRIGHT ROAD EXTENSION: -

PROVIDE END OF ROADWAY STREET BARRICADE

Know what's **below.**

ETC AS NECESSARY TO ENSURE THE SAFETY OF THE PUBLIC.

STANDARDS, PART 1926, SUBPART P, OR AS AMENDED.

BEGINNING CONSTRUCTION.

AUTHORITY SPECIFICATIONS.

Call before you dig.

AND ENSURING THAT ALL EXISTING UTILITIES ARE LOCATED PRIOR TO

2. CONTRACTOR IS RESPONSIBLE FOR PLACING BARRICADES, USING FLAGGERS,

3. ALL PAVEMENT CUTS, ASPHALT OR CONCRETE, ARE TO BE PLACED ACCORDING

4. SHORING SHALL BE CARRIED OUT IN ACCORDANCE WITH OSHA TRENCHING

TO STANDARDS OF THE DEPARTMENT OF TRANSPORTATION AND REGULATORY

1. CONTRACTOR IS FULLY RESPONSIBLE FOR CONTACTING APPROPRIATE PARTIES

[TOWN OF HUNTERSVILLE STD. NO. 706.1]

Total Acreage		76.13	
Total Square Feet		3316223	
Principal Uses (Z.O. Article 3)	Single Family Deta	4	
Building Type (Z.O. Article 3, 4, & 8.10)	Detached H	Detached House	
Zoning District (Z.O. Article 3)	TR		
Overlay District, If Applicable (Article 3 Z.O.)	NA	NA	
Percentage of Impervious Allowed (Article 3 Z.O.)	35%	35%	
Percentage of Impervious Shown	31%		
Lot Count	110		
Total Number of Dwellings (detached/attached)	110	110	
Mixed-use/# of live/work units	0		
Total Square Foot of Non-Residential	0		
Lot Size Range			
Number of Lots within Each Range	10000-12000 SF	51	
	12000-14000 SF	47	
	14000-16000 SF	10	
	> 16000 SF	2	
Lot widths range (Z.O. Article 3)			
Number of Lots within Each Range	≤70'	12	
	70'-85'	79	
	85'-100'	10	
	>100'	9	
Setbacks			
Front Setback	20'	20'	
Corner Setbacks	10'	10'	
Rear Setbacks	25'	25'	
Right Side Setback	8'	8'	
Left Side Setback	8'		
Watershed Information (Z.O. Article 3 and S.O. Se	ection 7.290)		
Is the property in a regulated watershed district?	No	100	
Watershed District	Clarke		

PROPOSED SITE LEGEND

PROJECT BOUNDARY

20' BUFFER YARD

80' LANDSCAPE

EXISTING R/W

PROPOSED ROW

PROPOSED LOT

PROPOSED 2'-0"

PROPOSED SIDEWALK

_

-0--0-

VALLEY GUTTER

PROPOSED

ROAD

A.D.A. RAMP

UTILITY ACCESS

SETBACK LINE

PROPOSED

LOT LINE



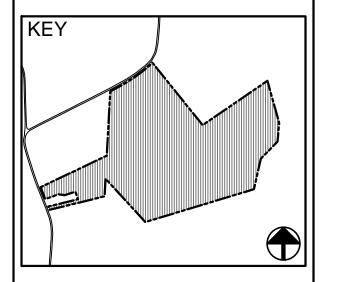


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POND BOUNDARY

PERENNIAL STREAM

CENTERLINE & TOP

20' STREAM SIDE

15' UPLAND ZONE

STREAM BUFFER

SIGHT TRIANGLE

PROPOSED

WATER LINE &

15'x15' SIGHT TRIANGLE

35'X35' SIGHT TRIANGLE

PROPOSED STREET SIGN

SPLIT-RAIL FENCE

VEHICLE BARRICADE

MAILBOX USPS CLUSTER BOX UNIT

PROPOSED NO PARKING SIGN

(NO DISTURB)

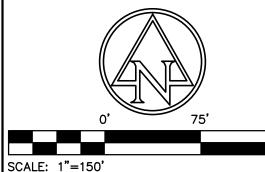
ZONE STREAM BUFFER

OF BANK

(NORMAL POOL)

STRUCTUREPOINT 2015.00721 PROJECT#

> **OVERALL SITE PLAN**



SECTION 7.280.

AN AREA OF APPROXIMATELY 0.19 ACRES BETWEEN THE PROPOSED ROW AND EXISTING ROW ALONG THE NORTHERN PROPERTY BOUNDARY AT ASBURY CHAPEL ROAD SHALL BE DEDICATED TO NCDOT. DRIVEWAYS SHALL EXTEND AT LEAST TWENTY (20) FEET FROM A PUBLIC RIGHT-OF-WAY OR EASEMENT FOR

A PUBLIC SIDEWALK. ALL AREAS DESIGNATED AS COMMON AND/OR URBAN OPEN SPACE SHALL BE OWNED AND MAINTAINED BY THE HOMEOWNERS ASSOCIATION AND DEDICATED FOR PERPETUITY.

ACCESS (INGRESS/EGRESS) LOCATION AND DESIGN SHOWN ON THIS SKETCH PLAN ARE SUBJECT TO ACHIEVING VERTICAL AND HORIZONTAL SIGHT DISTANCES, TURN LANE IMPROVEMENTS (INCLUDING RIGHT-OF-WAY), AND INTERSECTION WITH ADJACENT AND OPPOSING ACCESS POINTS. MODIFICATIONS TO PLAN MAY RESULT. 9. LARGE MATURING TREES WILL BE PLANTED 40' O/C WITHIN THE REQUIRED PLANTING STRIP ALONG ALL

STREETS WITH THE EXCEPTION OF RURAL PARKWAYS WHERE EXISTING TREES CAN SATISFY THE STREET TREE REQUIREMENT. SMALL MATURING TREES ARE TO BE USED WHERE OVERHEAD POWER LINES EXIST. 10. MAIL DELIVERY SHALL BE CARRIED OUT BY USPS, VIA A SERIES OF CLUSTERBOX UNITS, OR MAIL KIOSKS, SPACED STRATEGICALLY THROUGHOUT THE DEVELOPMENT TO FACILITATE MORE EFFICIENT MAIL DELIVERY AND PICK-UP.

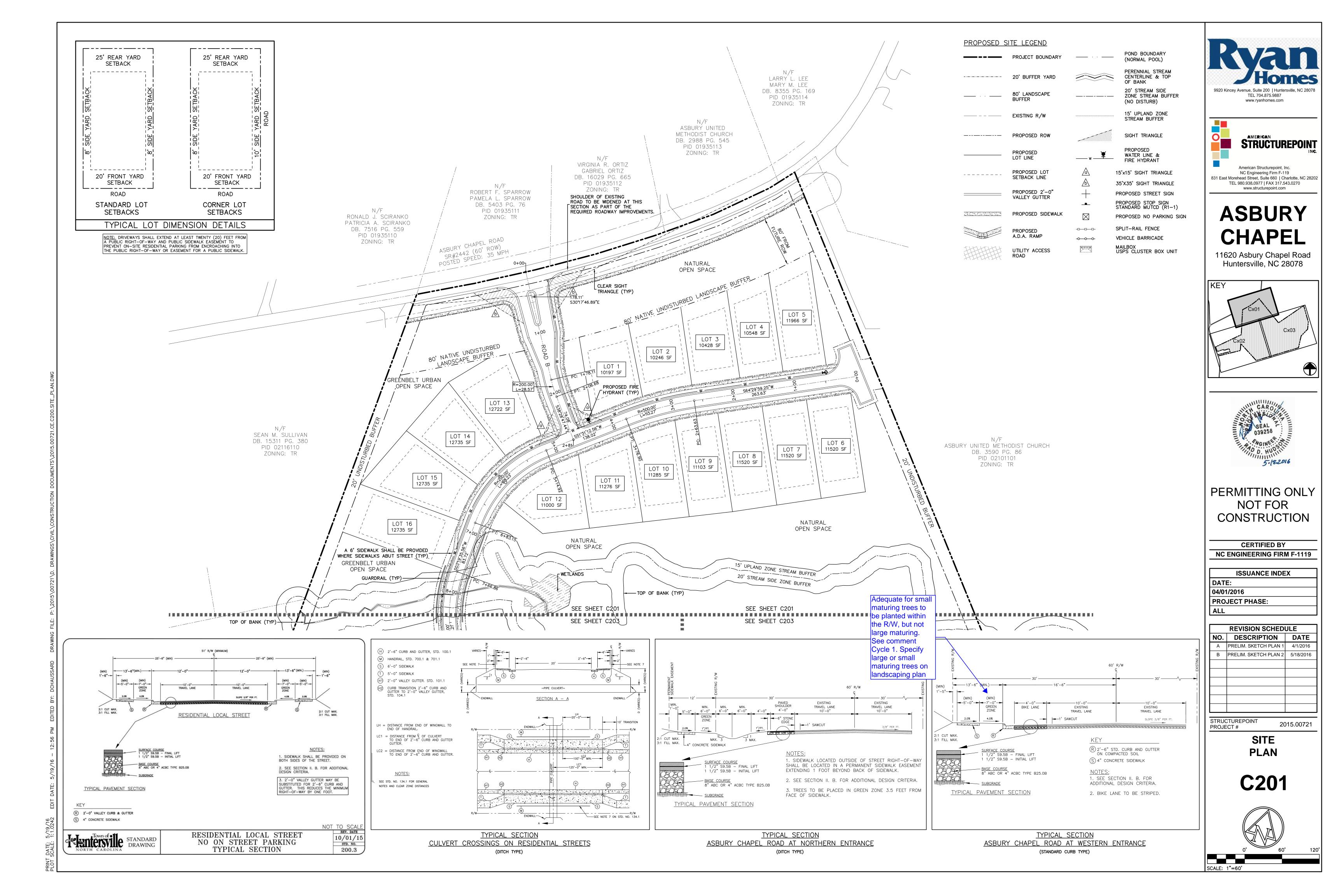
PENDING APPROVAL, GARBAGE COLLECTION SERVICES WILL BE PROVIDED BY THE TOWN OF HUNTERSVILLE. 12. EXCEPT AS SPECIFICALLY PROVIDED IN THE RURAL AND TRANSITIONAL DISTRICTS, DETACHED HOUSE TYPES AND TWO-UNIT ATTACHED HOUSE TYPES SHALL BE REQUIRED TO HAVE A LOT WIDTH GREATER THAN 50' FEET. DEVELOPERS SHALL HAVE THE OPTION TO PROVIDE LOTS WIDTHS 50' FEET WIDE OR LESS WHEN ALLEY ACCESS ALLOWING ON-SITE PARKING IS PROVIDED FOR DETACHED HOUSE TYPES AND TWO-UNIT ATTACHED HOUSE TYPES. FURTHER, DEVELOPERS SHALL ALSO HAVE THE OPTION TO APPLY FOR A CONDITIONAL DISTRICT REZONING TO CREATE LOT WIDTHS OF 50 FEET WIDE OR LESS WITHOUT AN ALLEY PROVIDED IT CAN DEMONSTRATED THE DEVELOPMENT CAN ACCOMMODATE ON-STREET PARKING, MEET ALL THE PROVISIONS OF THE ZONING ORDINANCE INCLUDING INSTALLATION OF STREET TREES, AND DEMONSTRATE

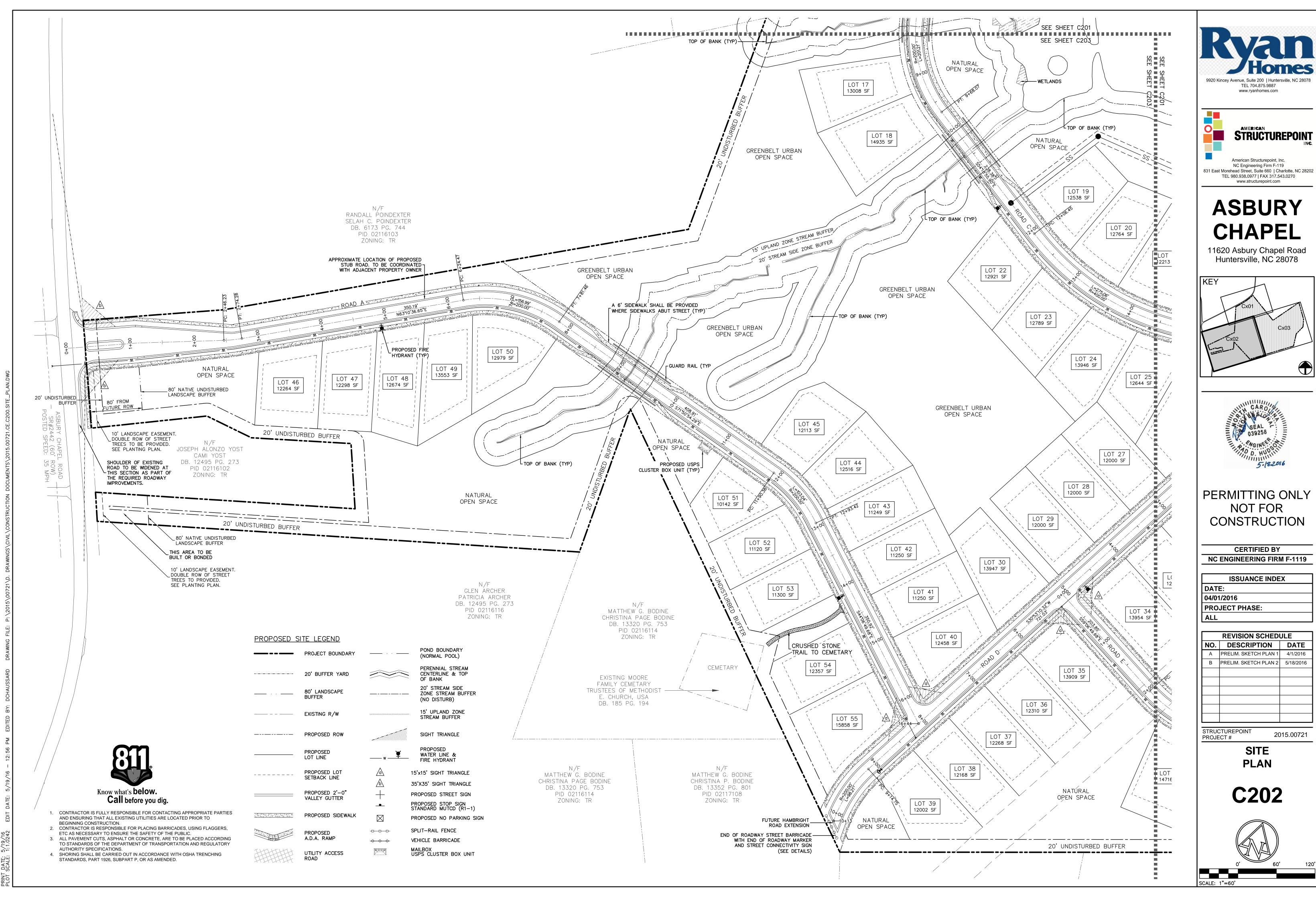
PUBLIC SERVICES SUCH AS TRASH PICK-UP CAN BE PROVIDED. ALLEY ACCESS FOR ON-SITE PARKING IS STRONGLY ENCOURAGED FOR THREE-UNIT OR MORE ATTACHED HOUSE TYPES.

DIRECT ACCESS FROM LOTS TO ASBURY CHAPEL ROAD IS PROHIBITED.

SPACING OF PROPOSED FIRE HYDRANTS NOT TO EXCEED 750 FEET FROM THE MOST REMOTE POINT OF ANY PROPOSED BUILDING, AS THE FIRE TRUCK TRAVELS. SEE PLAN FOR PLACEMENT OF FIRE HYDRANTS.

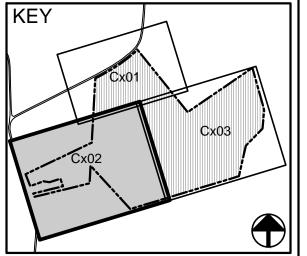
16. PLEASE SEE OPEN SPACE PLAN (SHEET C250) FOR OPEN SPACE LAYOUT, IMPROVEMENTS, AND ACREAGES.





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